



Scale in Feet  
0 250 500

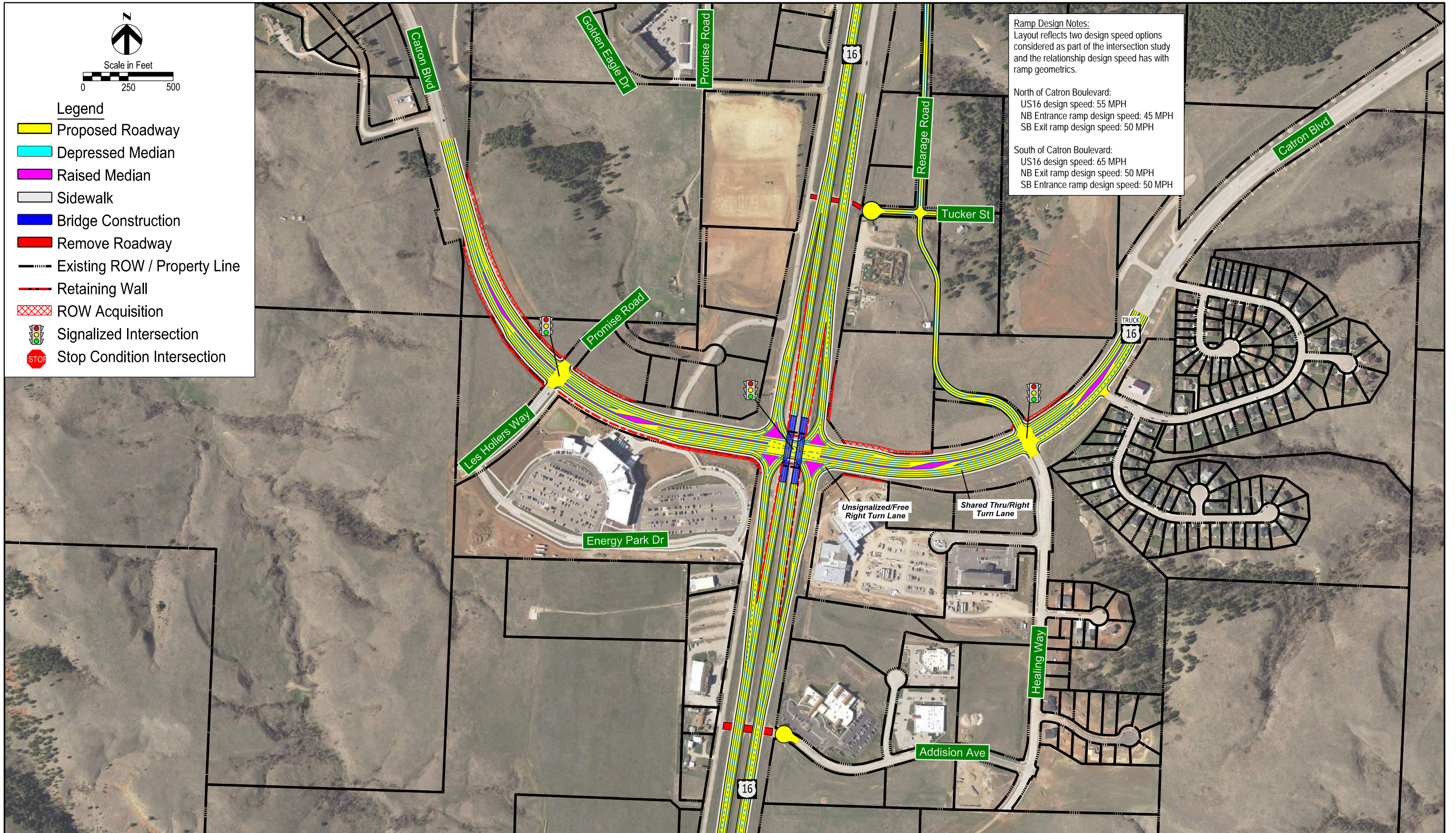
**Legend**

- Proposed Roadway
- Depressed Median
- Raised Median
- Sidewalk
- Bridge Construction
- Remove Roadway
- Existing ROW / Property Line
- Retaining Wall
- ROW Acquisition
- Signalized Intersection
- Stop Condition Intersection

**Ramp Design Notes:**  
Layout reflects two design speed options considered as part of the intersection study and the relationship design speed has with ramp geometrics.

North of Catron Boulevard:  
US16 design speed: 55 MPH  
NB Entrance ramp design speed: 45 MPH  
SB Exit ramp design speed: 50 MPH

South of Catron Boulevard:  
US16 design speed: 65 MPH  
NB Exit ramp design speed: 50 MPH  
SB Entrance ramp design speed: 50 MPH



Drawn By:  
Date:  
Chkd By:  
Date:  
Revision:



Alternative 1.1a  
SPI with Separated, Free NB and SB Right Turn Lanes  
US16 Corridor Study

Figure

Rapid City, SD



Scale in Feet  
0 250 500

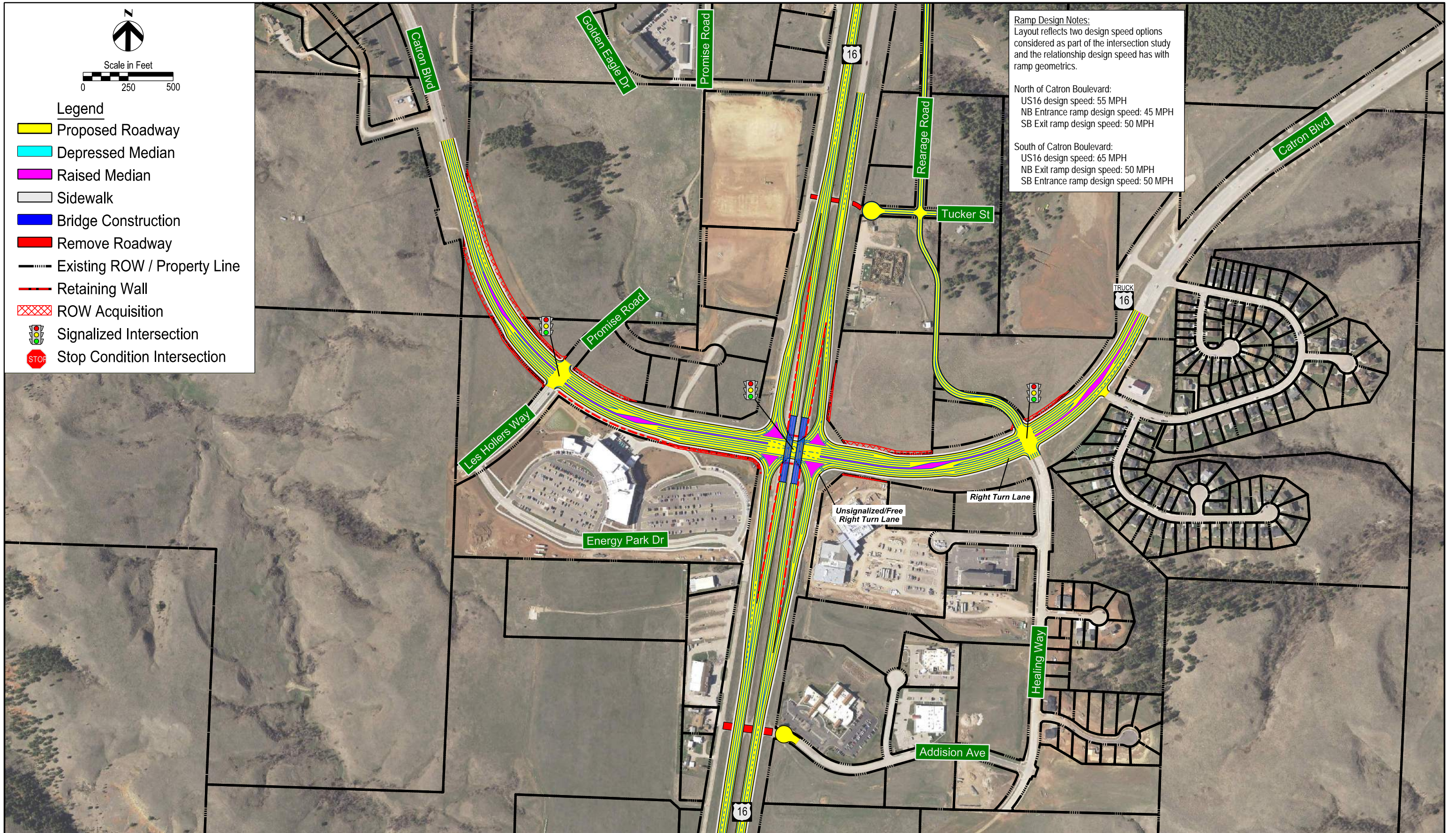
**Legend**

- Proposed Roadway
- Depressed Median
- Raised Median
- Sidewalk
- Bridge Construction
- Remove Roadway
- Existing ROW / Property Line
- Retaining Wall
- ROW Acquisition
- Signalized Intersection
- Stop Condition Intersection

**Ramp Design Notes:**  
 Layout reflects two design speed options considered as part of the intersection study and the relationship design speed has with ramp geometrics.

North of Catron Boulevard:  
 US16 design speed: 55 MPH  
 NB Entrance ramp design speed: 45 MPH  
 SB Exit ramp design speed: 50 MPH

South of Catron Boulevard:  
 US16 design speed: 65 MPH  
 NB Exit ramp design speed: 50 MPH  
 SB Entrance ramp design speed: 50 MPH



Drawn By:  
 Date:  
 Chkd By:  
 Date:  
 Revision:



Alternative 1.1b  
 SPI with Separated, Free NB and SB Right Turn Lanes with EB Right Turn Lane at Healing Way  
 US16 Corridor Study

Figure

Rapid City, SD



Scale in Feet  
0 250 500

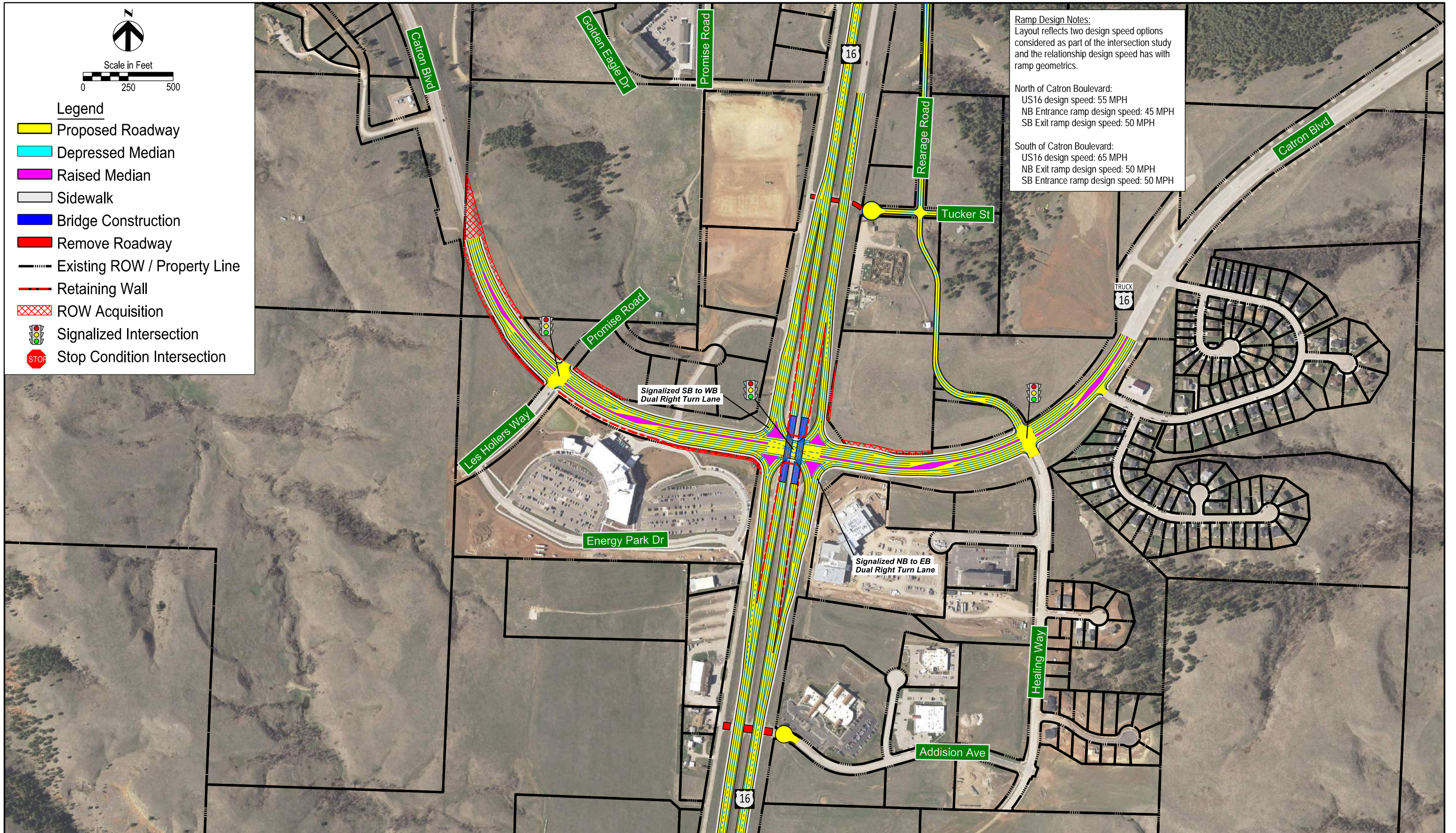
**Legend**

- Proposed Roadway
- Depressed Median
- Raised Median
- Sidewalk
- Bridge Construction
- Remove Roadway
- Existing ROW / Property Line
- Retaining Wall
- ROW Acquisition
- Signalized Intersection
- Stop Condition Intersection

**Ramp Design Notes:**  
Layout reflects two design speed options considered as part of the intersection study and the relationship design speed has with ramp geometrics.

North of Catron Boulevard:  
US16 design speed: 55 MPH  
NB Entrance ramp design speed: 45 MPH  
SB Exit ramp design speed: 50 MPH

South of Catron Boulevard:  
US16 design speed: 65 MPH  
NB Exit ramp design speed: 50 MPH  
SB Entrance ramp design speed: 50 MPH



Drawn By:  
Date:  
Chkd By:  
Date:  
Revision:



Alternative Scenario 1.2  
SPI with Signalized NB and SB Dual Right Turn Lanes and EB Right Turn Lane at Healing Way  
US16 Corridor Study

Figure

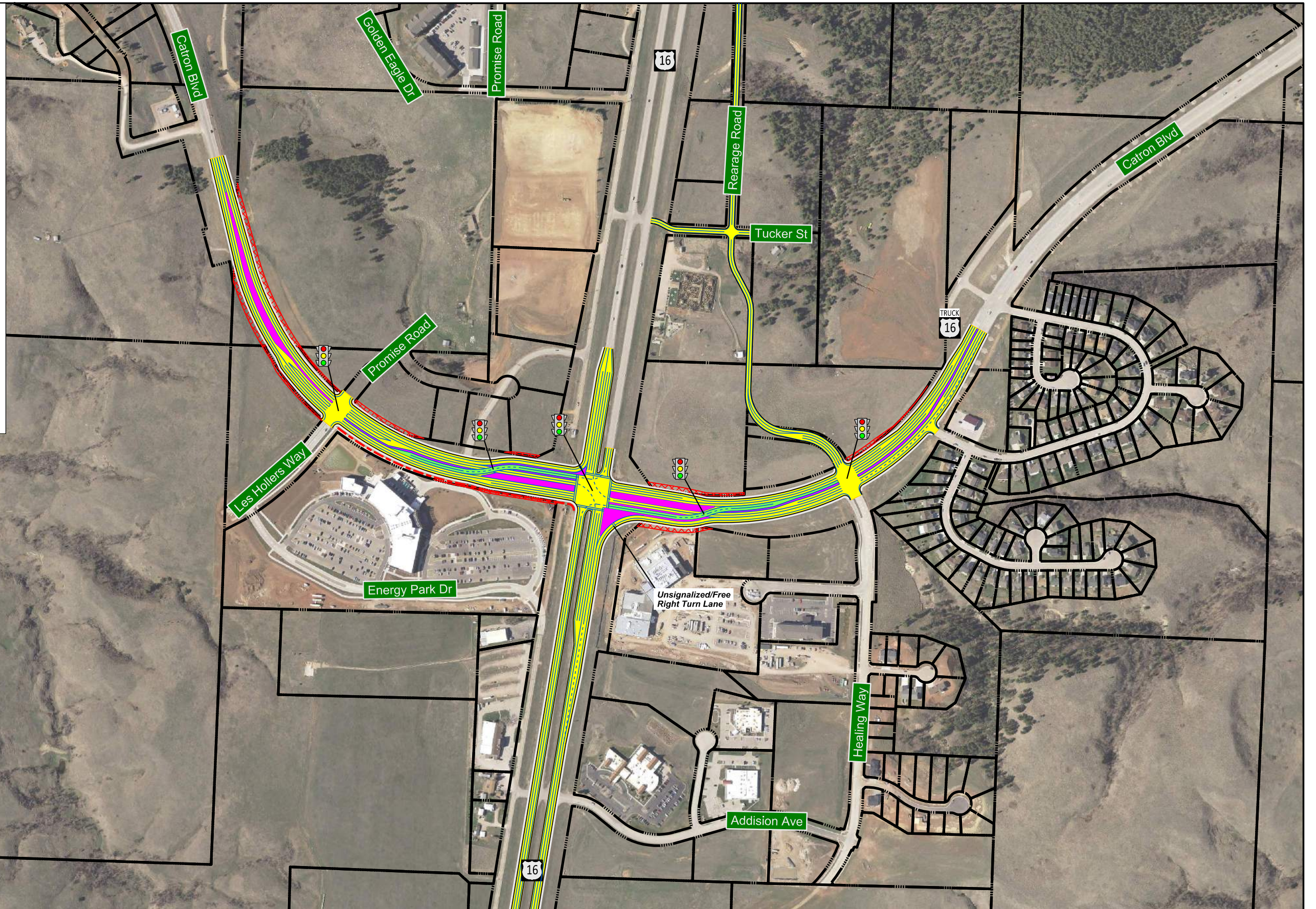
Rapid City, SD



Scale in Feet  
0 250 500

Legend

- Proposed Roadway
- Displaced Left
- Depressed Median
- Raised Median
- Sidewalk
- Bridge Construction
- Remove Roadway
- Existing ROW / Property Line
- Retaining Wall
- ROW Acquisition
- Signalized Intersection
- Stop Condition Intersection



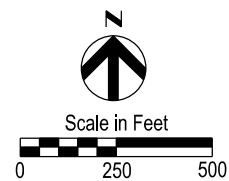
Drawn By:  
Date:  
Chkd By:  
Date:  
Revision:



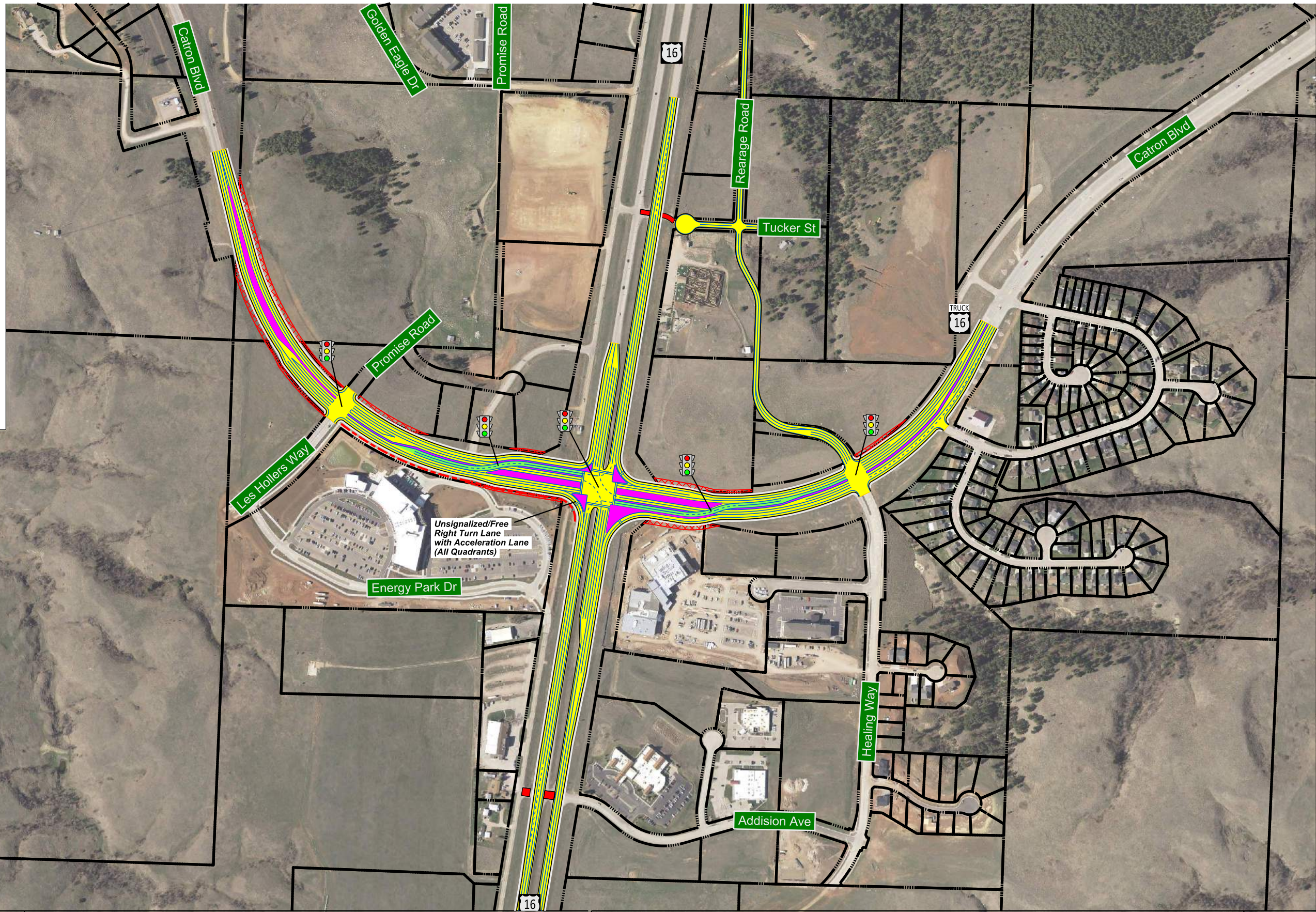
Alternative 2.1a  
DLT with Separated, Free NB and SB Right Turn Lanes  
US16 Corridor Study

Figure

Rapid City, SD



- Legend**
- Proposed Roadway
  - Displaced Left
  - Depressed Median
  - Raised Median
  - Sidewalk
  - Bridge Construction
  - Remove Roadway
  - Existing ROW / Property Line
  - Retaining Wall
  - ROW Acquisition
  - Signalized Intersection
  - Stop Condition Intersection



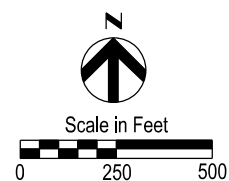
Drawn By:  
Date:  
Chkd By:  
Date:  
Revision:



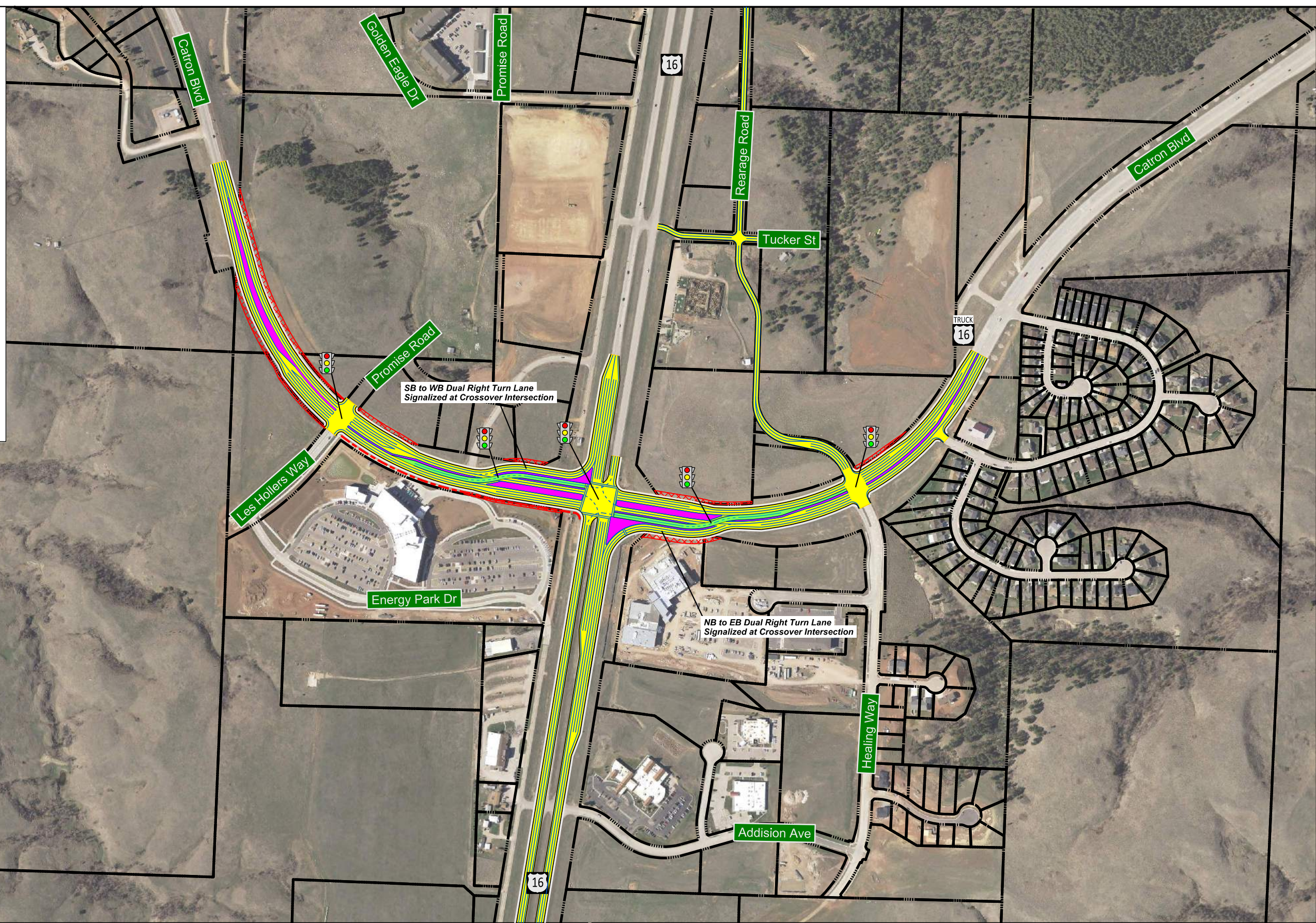
Alternative 2.1b  
DLT with Separated, Free Right Turn Lanes at Main Intersection (all Quadrants)  
US16 Corridor Study

Figure

Rapid City, SD



- Legend**
- Proposed Roadway
  - Displaced Left
  - Depressed Median
  - Raised Median
  - Sidewalk
  - Bridge Construction
  - Remove Roadway
  - Existing ROW / Property Line
  - Retaining Wall
  - ROW Acquisition
  - Signalized Intersection
  - Stop Condition Intersection



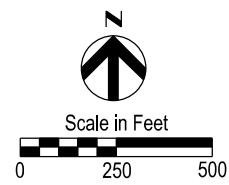
Drawn By:  
Date:  
Chkd By:  
Date:  
Revision:



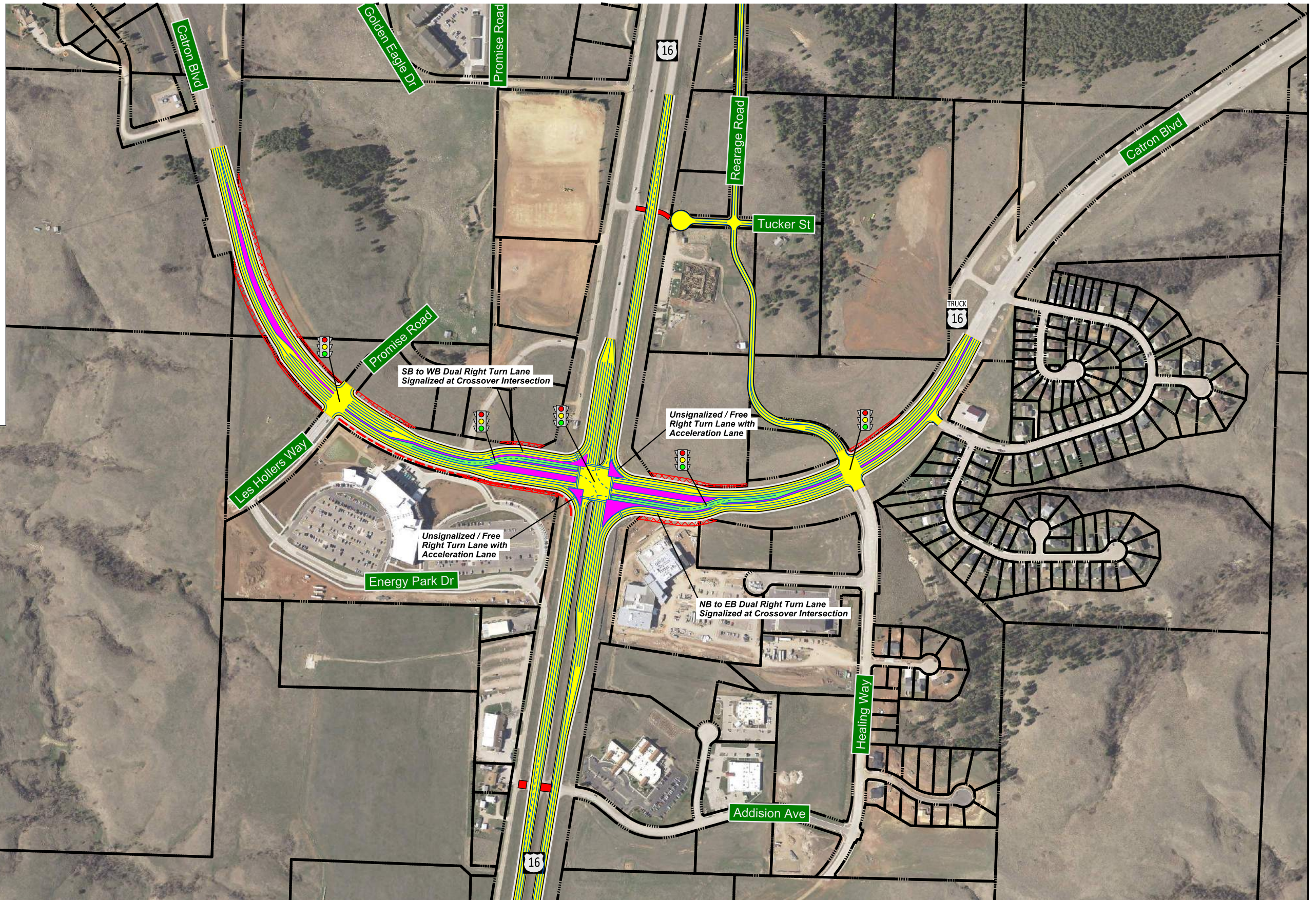
Alternative 2.2a  
DLT with Signalized Right Turn Lanes at Main Intersection (all Quadrants)  
US16 Corridor Study

Figure

Rapid City, SD



- Legend**
- Proposed Roadway
  - Displaced Left
  - Depressed Median
  - Raised Median
  - Sidewalk
  - Bridge Construction
  - Remove Roadway
  - Existing ROW / Property Line
  - Retaining Wall
  - ROW Acquisition
  - Signalized Intersection
  - Stop Condition Intersection



Drawn By:  
Date:  
Chkd By:  
Date:  
Revision:



Alternative 2.2b  
DLT with NB and SB Signalized Right Turn Lanes, Free EB and WB Right Turn Lanes  
US16 Corridor Study

Figure

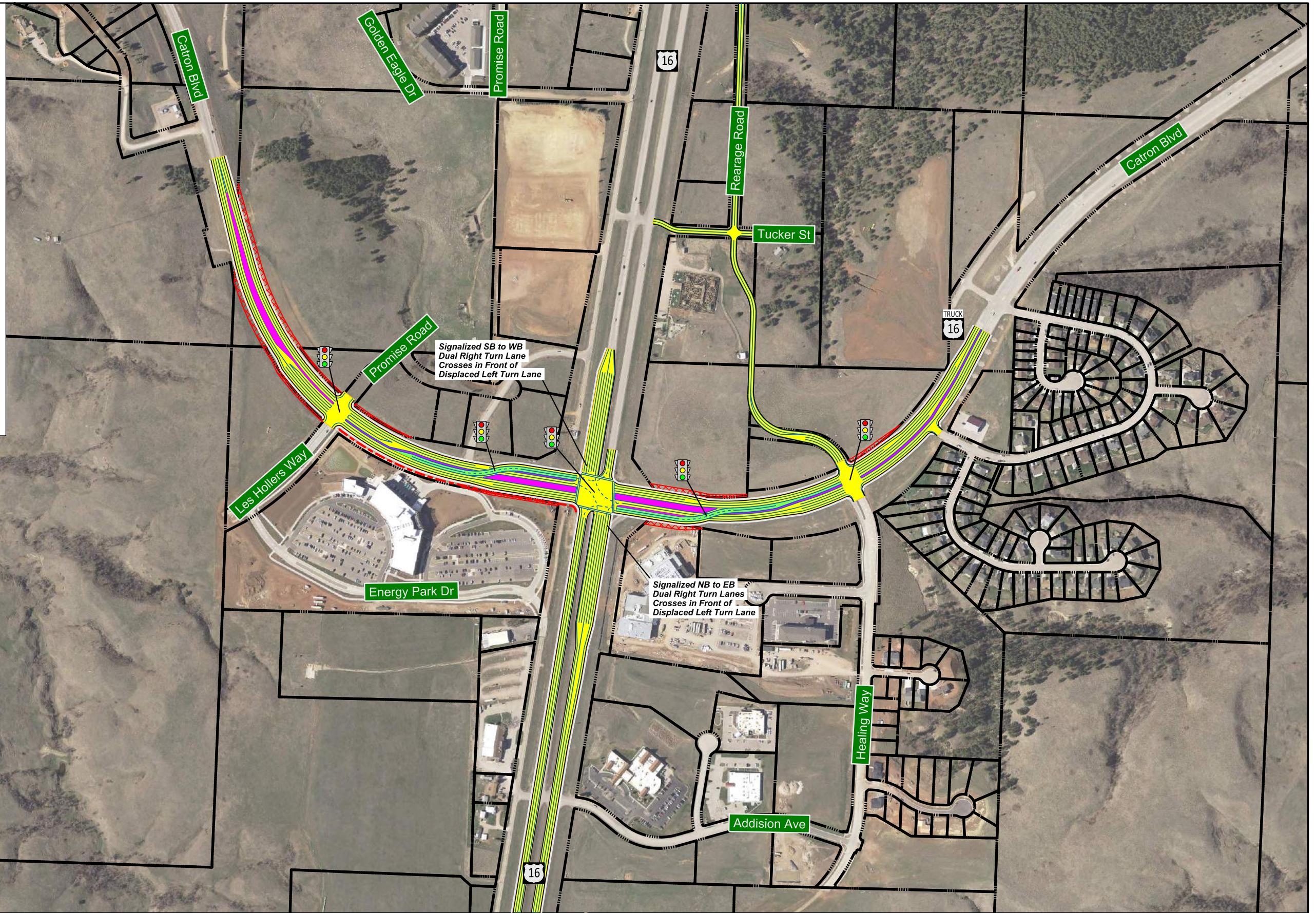
Rapid City, SD



Scale in Feet  
0 250 500

Legend

- Proposed Roadway
- Displaced Left
- Depressed Median
- Raised Median
- Sidewalk
- Bridge Construction
- Remove Roadway
- Existing ROW / Property Line
- Retaining Wall
- ROW Acquisition
- Signalized Intersection
- Stop Condition Intersection



Drawn By:  
Date:  
Chkd By:  
Date:  
Revision:



Alternative 2.3a  
DLT with Unseparated, Signalized Right Turn Lanes at Main Intersection  
US16 Corridor Study

Figure

Rapid City, SD